

# ROSWELL

## EAST-WEST ALLEY MASTER PLAN



INTEGRATING ALLEYWAYS INTO THE FABRIC OF HISTORIC ROSWELL  
2015





**“THE AIR WAS SOFT, THE STARS SO FINE,  
THE PROMISE OF EVERY COBBLED ALLEY  
SO GREAT THAT I THOUGHT I WAS IN A  
DREAM.”**

**- JACK KEROUAC**



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Jere Wood, Mayor  
Rich Dippolito  
Becky Wynn  
Betty Price  
Kent Igleheart  
Jerry Orlans  
Nancy Diamond

**Historic Preservation Commission**

Tony Landers, Chair  
Judy Meer, Vice Chair  
Alex Paulson  
Bill Bruce  
Richard Hallberg  
Tom Lynch  
Chad Hagan  
Cecilia Fly, Historical Society

**City of Roswell Staff**

Kay Love, City Administrator  
Alice Wakefield, Community Development Director  
Mark Wolff, Deputy Community Development Director  
Brad Townsend, Planning and Zoning Director  
Courtney Lankford, Planner II (Project Manager)  
Steve Acenbrak, Transportation Director  
Clyde Stricklin, Land Development Manager  
Dan Skalsky, Environmental/Public Works Director  
Alice Champagne, Deputy Environmental/Public Works Director

**Roswell, Inc**

Steve Stroud

**Brown and Caldwell**

Laurie J. Hawks  
Neil Schaap

**Pond & Company**

Matthew Wilder  
Andrew White



# INTRODUCTION

In 2014, the City of Roswell Community Development Department, in an effort to support an active, safe, and economically vital historic downtown district, identified existing alleyways and side streets off Canton Street as features for study and improvement. Pond & Company was contracted to facilitate the creation of an Alley Master Plan with input from the community. This report is the result of that effort.

Because of the recent adoption of a new Unified Development Code (UDC) and Design Guidelines, the City of Roswell has new regulatory and form-based tools to significantly enhance the visual pedestrian experience in its historic downtown district. Many of the goals and guiding principles outlined in the UDC also apply to this Master Planning effort, among which are:

- 1) To promote and protect the public health, safety, and welfare.
- 2) To promote environmental, economic, and social sustainability.
- 3) To protect the physical environment, historic character and natural resources for all citizens
- 4) To preserve, protect and enhance the City's employment base
- 5) To improve connectivity and build upon the success of the walkable village concept of Canton Street

The Master Plan will be divided into primary and secondary focus areas, as shown in Figure 1 on page 4 of this report. The primary focus areas consist of predominately undeveloped streetscape connections, and include: East Alley, West Alley, and Webb Street East. The secondary focus areas consist of predominately developed streetscape connections (with the exception of Webb Street West) and include: Elizabeth Way, Mimosa Boulevard, Canton Street, and Webb Street West.

Although this master plan is known as the East-West Alley Master Plan, its limits encompass more than the two alleyways referenced in the title. In addition to the two service alleys, East Alley and West Alley, the study area also comprises five streets, which are designated to remain streets. It is not the purpose or intent of this master plan to treat these streets as alleyways. Recommendations for each corridor will be developed specific to its use, context, and desired final condition. It is hoped that this plan will fully integrate the alleys with the streetscape of Historic Downtown Roswell.

Stormwater management needs in the study area are addressed in a concurrent study by Roswell Public Works under separate contract with Brown & Caldwell. Their findings are considered to be an important component of this planning effort. Representatives from Public Works and Brown & Caldwell will be involved at every stage of this master planning process.

This master planning effort will employ a comprehensive design process, broken up into three distinct phases.

## Phase 1: Inventory and Analysis

- Meet with City staff to gather information pertinent to the design process.
- Visit and photograph the project area and develop a site inventory for the project area.
- Review local and state design controls related to functional and aesthetic improvements.
- Review and prepare an assessment of the collected data.

## Phase 2: Design Charrette

- Meet with City staff to develop the charrette format.
- Prepare charrette materials and presentation boards
- Facilitate a full-day design charrette with city staff, business owners, heritage professionals, and other stakeholders.
- Compile results of charrette and develop a concept plan and design standards based on those results
- Meet with elected officials, the Historic Preservation Commission, city staff, and other primary stakeholders to review concept plans
- City of Roswell will host a Public Open House regarding the concept plans

## Phase 3: Develop Corridor Master Plan

- Prepare draft master plan, including a cohesive streetscape plan with enlargements, typical sections, materials palette, and opinion of probable cost
- Meet with city staff, elected officials, and the Historic Preservation Commission to review draft master plan and address comments
- City of Roswell will host a Public Open House regarding the master plan
- Present revised master plan to Mayor and City Council at a Work Session
- Address final comments and issue final corridor master plan for adoption or acceptance.

## What is an alley?

Although this plan focuses on some corridors that are not alleyways, it is helpful to define a common understanding of what constitutes an alley. For the purposes of this Master Plan, we are interested in the interstitial, secondary circulation lanes between or behind buildings. They are most commonly used for refuse access, deliveries, utility service, parking, and other “back door” functions. They may also be used by pedestrians as shortcuts, and they allow light and air into the rear of buildings.

While the corridors in this study vary in width, materials, and use, it is the goal of this Master Plan to explore creative options for reuse and improvement so as to enhance the network of active, safe, and social spaces in Historic Roswell.





Figure 1: Study Area Map



# EXISTING CONDITIONS: INVENTORY

The extent of the study area for this master plan can be seen in Study Areas Map (Figure 1) opposite this page. The area of focus consists of seven distinct corridors, all of which create a pedestrian and vehicular network throughout Historic Downtown Roswell. The study area roughly extends from Magnolia Street on the southern edge to Norcross Street on the northern edge, and from Alpharetta Street on the eastern edge to the end of Webb Street on the western edge.

Three maps were developed for the study area to show existing conditions, which are included in this report, and discussed further below.

The Land Use Map (Figure 2) shows differences in land use and ownership in the study area. Each area has unique constraints and must respond to adjacent land use. Most of the land in this study area falls within the historic district boundary and is used for private commercial enterprise, with many small businesses and restaurants occupying the historic buildings lining Canton Street and backing into the alleys. Webb Street West, however, lies mainly outside the historic district, with the exception of some of its northern parcels. It is characterized by low-density residential land use. Due to its proximity to Canton Street, development pressure has been heavy in this area, and new townhouse developments are currently under construction on part of this stretch of road. The townhome developments currently under construction on Webb Street West are within the historic district.

The Circulation Map (Figure 3) helps to illustrate vehicular and pedestrian circulation throughout the study area by documenting the direction of traffic and the presence or absence of pedestrian and bicycle facilities, such as bike racks, sidewalks, crosswalks, and bicycle lanes. Striped parking spaces for cars are quantified on this plan as well.

The Infrastructure Map (Figure 4) was created to understand the physical structures in the study area, including walls, fences, utility lines, storm drainage infrastructure, refuse storage, outdoor dining areas, trees, and other physical features in the landscape.

This inventory of existing conditions was used in both the analysis and planning of the study area and were studied as exhibits in the community charrette.



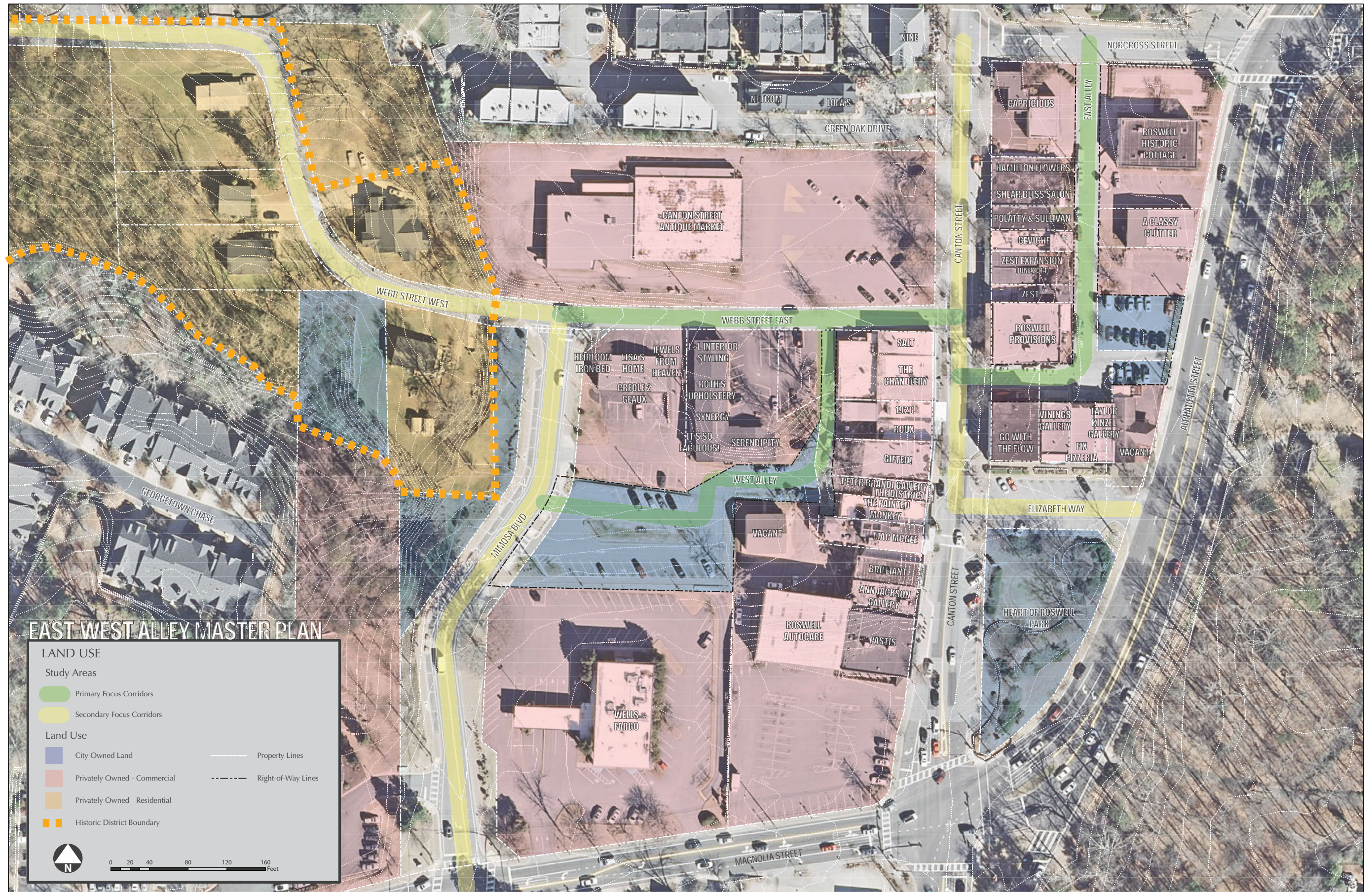


Figure 2: Land Use Map





Figure 3: Circulation Map





Figure 4: Infrastructure Map



# EXISTING CONDITIONS: ASSETS, CHALLENGES & OPPORTUNITIES

Prior to design, the consultant team developed a detailed analysis once the initial inventory had been completed. Each corridor was considered separately during this first phase of analysis (see pages 10-12).

As the analysis developed, common themes quickly emerged for the entire study area. These trends were teased apart from the detailed analysis and given more weight in a photographic analysis (see pages 13-15).

It is also worth mentioning that the City of Roswell passed a new Unified Development Code and accompanying Design Guidelines in 2014, which required examination and consideration. All recommendations for the East-West Alley Master Plan study area will comply with this legislation, the implications of which are also enumerated on page 12 of this report.

The final phase of analysis constitutes an inventory of “alleyways worth a second glance” (see pages 16-17). These alleyways possess characteristics that may be desirable in Historic Downtown Roswell and act as design precedents for the East-West Alley planning effort. Elements common among them include: defined edges, decorative pavement, consistent landscape, consistent furnishings, outdoor social spaces, limited vehicular access, and rainwater infiltration devices.

These analyses were used during the charrette and concept plan development phases of this project and helped to establish the vision for the plan going forward.





East Alley is probably the most activity-dense alley in the historic downtown. Flanked on all sides by established and popular businesses, the alley serves as a public parking lot, two-way public street, service access zone, overhead utility route, pedestrian corridor, outdoor dining spot, and waste disposal area. The multitude of uses in this relatively narrow corridor has contributed to a rather chaotic and fragmented urban zone.

## ASSETS

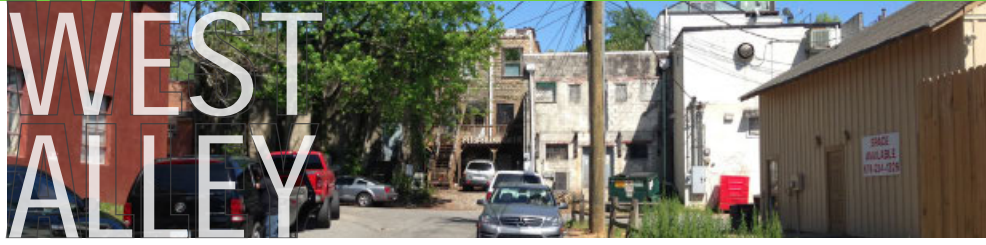
- Strong connection to Canton Street
- Extensive retail and restaurant frontage
- Existing activation by outdoor dining spaces
- Linkage with regional transit
- Articulated building footprints create opportunities for courtyards
- Significant historic resources
- Trash/dumpster screening & consolidation has already begun
- 1 to 2-story buildings contribute to a warm, light-filled environment
- Open edge on the east for transparency
- Alley feels bright, wide, and textured
- Already has a “brand” – East Alley is a cool name

## CHALLENGES

- Dangerous sight lines exiting East Alley onto Canton street
- Integrated dumpster service functioning less effectively than hoped
- Unsightly overhead utilities
- Limited parking capacity
- Tenants fill up available parking
- Frequent vehicular use
- Frequent need for commercial vehicle access
- Rainwater drainage capacity is insufficient
- Untreated rainwater degrades river
- Lack of safe/separate pedestrian facilities
- Lack of pedestrian-scale lighting
- Poor pavement condition and materials

## OPPORTUNITIES

- Retail and dining courtyards and plazas
- Partial or part-time closure to through traffic
- Expand building square footage
- Possible one-way reconfiguration
- Rainwater features that enhance water quality and improve aesthetics
- Formalized pedestrian circulation
- On-alley business entrances
- Unique destination for shopping and dining
- Integrate public art
- Utility burial
- Nightlife experience with decorative lighting and restaurant expansion
- Cooperative parking arrangements
- Embrace historic character as a theme
- Enhanced landscape treatments
- Increase East Alley “brand” with custom signage
- Formalize loading zones



West Alley is a narrow two-way street, principally used as a service area for businesses along Webb Street and the west side of Canton Street. It currently has only a few commercial storefront interfaces near its intersection with Mimosa, and handles “rear door” functions for all other businesses. Although the potential exists for West Alley to eventually have the same commercial draw and pedestrian enhancements envisioned for East Alley, a more urgent need is basic reorganization for more efficient delivery of services and enhanced functionality.

## ASSETS

- Strong connection to Canton Street
- Articulated building footprints create opportunities for courtyards
- Significant historic resources
- Trash/dumpster screening & consolidation has begun
- Lower grade elevation contributes to feeling of quiet separation from busier parts of downtown
- Recently redeveloped public parking lot on south edge

## CHALLENGES

- Limited landscape improvements
- Rainwater drainage capacity is insufficient
- Untreated rainwater degrades river
- Ill-defined edges
- No safe/separate pedestrian facilities
- Unsightly overhead utilities
- Integrated trash/dumpster service functioning less effectively than hoped
- Unsightly/fragmented waste disposal areas
- Extensive trash service needs
- Frequent need for commercial vehicle access
- Poor pavement condition and materials
- Lack of pedestrian scale lighting
- Lower grade elevation can contribute to isolated feeling
- Limited retail and restaurant interface

## OPPORTUNITIES

- Open edges create opportunities for infill development and redevelopment
- Continue to consolidate/strengthen combined dumpster service and resolve maintenance issues
- Future potential for restaurant and retail courtyards and plazas
- Expanded building square footage
- Strengthen interface with Webb Street East, Mimosa Boulevard, and Magnolia Street
- Rainwater features that enhance water quality and improve aesthetics
- Formalize pedestrian circulation
- Utility burial
- Embrace historic character as a theme
- Begin branding effort for West Alley
- Formalize loading zones

## PRIMARY FOCUS AREAS



Webb Street East is a two-way street that connects Mimosa Boulevard with Canton Street. It is probably used most as a vehicular thoroughway and pedestrian connection from public parking areas to Canton Street. It has limited retail frontage on its south side that is not served by any formal pedestrian facilities. The north side of the street is not activated at all, consisting of a grass bank on its west end, and a medium-sized surface parking lot on its east end, which transitions to shared valet parking during peak hours.

## ASSETS

- Strong connection to Canton Street
- Street width accommodates parallel parking on east end
- Some historic resources
- Limited need for service or delivery access
- Limited need for waste disposal
- services
- Open edges create flexibility and prime redevelopment opportunities
- Connection of historic downtown to public parking facilities creates pathway that is frequently used
- Street feels bright and wide

## CHALLENGES

- Limited retail frontage
- Rainwater drainage capacity is insufficient
- Untreated rainwater degrades river
- Lack of safe/separate pedestrian facilities
- Lack of lighting
- Lack of mature canopy and other landscape treatments
- Overhead utilities and grade change interfere with activating north side of street
- Parallel parking spaces are unmarked and otherwise not formalized
- Inadequate vehicular sightlines at intersection with Canton Street

## OPPORTUNITIES

- Retail courtyards
- Opportunity for formalized pedestrian circulation and enhanced streetscape treatment
- Rainwater features that enhance water quality and improve aesthetics
- Integrate public art
- Opportunity for utility burial
- Opportunities for more formal parking arrangements
- Embrace historic character as a theme
- Green spaces create opportunities for enhanced landscape treatments and pocket parks
- Large open spaces create opportunity for infill or redevelopment
- Opportunity to create formal gateway or corridor from public parking facilities along West Alley and Mimosa Boulevard to historic downtown





Mimosa Boulevard is a recently-constructed two-way street that acts as an extension of an existing historic road of the same name just south of the study area. It connects Magnolia Street with Webb Street and West Alley, but has neither retail nor restaurant frontage. When it was constructed in 2008/2009, the City took full advantage of its proximity to the historic downtown core and outfitted the street with full streetscape facilities, including marked and curbed parallel parking spots, lighting, unit paver sidewalks, bicycle lanes, crosswalks, landscape, and a dedicated parking lot that serves the historic downtown area.

## ASSETS

- Good connection to Historic Downtown Roswell
- Park-like landscaped scenic character on west side of street
- Views of historic buildings
- High level of streetscape treatment
- Safe and separate bicycle and pedestrian facilities
- Formal, marked parking facilities
- Well-lit with city standard fixtures
- Organized/tidy overhead utility lines
- Some mature preserved trees create shaded areas

## CHALLENGES

- No commercial or residential frontage
- East side of street mainly characterized by asphaltic pavement and surface parking
- Rainwater drainage capacity is insufficient
- Untreated rainwater degrades river

## OPPORTUNITIES

- Open edges create opportunities for infill development
- Strengthen interface with Webb Street East and West Alley
- Incorporate wayfinding signage
- Introduce rainwater features that enhance water quality and improve aesthetics



Elizabeth Way currently functions as a one-way street, angled parking lot, and sidewalk connecting Alpharetta Street and Canton Street. It features five historic row storefronts on its north side and Heart of Roswell Park, a half-acre public green space, on its south side.

## ASSETS

- Strong connection to Canton Street
- Wide corridor with structured, shared parking already in place
- Existing retail and proposed restaurant frontage
- Linkage with regional transit
- Significant historic resources
- No significant waste disposal operations are present
- No significant need for service or delivery vehicles
- Separate curbed pedestrian facilities
- Low structures contribute to a warm, light-filled environment
- Open edges for transparency
- Large canopy trees on the south edge and east end create shaded zones
- Pedestrian pavement material contributes to historic downtown feeling
- Landscape and wall buffer in place between storefronts and parking facilities
- No unsightly overhead utilities

## CHALLENGES

- Frequent vehicular use versus the need to maintain parking capacity
- Lack of pedestrian-scale lighting
- Rainwater drainage capacity is insufficient
- Untreated rainwater degrades the river
- Perceived poor visibility for storefronts and lack of pedestrian traffic
- Garden wall buffer limits opportunities for outdoor dining and retail plazas due to space constraints
- Rainwater features that enhance

## OPPORTUNITIES

- water quality and improve aesthetics
- Opportunity for pedestrian scale lighting
- Integrate public art
- Embrace historic character as a theme
- Reconfigure garden wall buffer to allow space for outdoor retail and restaurant areas
- Add bicycle facilities/storage
- Strengthen integration of regional bus stops



Canton Street is the principle artery of the historic downtown core and boasts many successful businesses, an active street scene both day and night, and a high order of streetscape treatment, including street trees, lighting, traffic calming measures, and rich materiality. When studying the alleyways of historic Roswell, it is impossible to ignore this stretch of Canton Street. It directly interfaces with three of the corridors under investigation, and it houses most of the businesses that draw people to this area in large numbers.

## ASSETS

- Principle artery of Historic Downtown Roswell
- Streetscape improvements already in place
- Extensive retail and restaurant frontage with outdoor dining
- Significant historic resources
- No significant waste disposal operations
- No service need aside from maintenance
- Pedestrian pavement materials contribute to historic downtown feeling
- 1 to 2-story buildings contribute to a warm, light-filled environment
- Street feels bright and textured
- Mature canopy trees along corridor create shaded zones
- No unsightly overhead utilities
- Active street life both day and night

## CHALLENGES

- Parking capacity is a challenge in and around Canton Street
- Heavy & steady vehicular use
- Limited space for pedestrians to walk due to sidewalk cafes
- Alterations to streetscape due to relocated street lights, parallel parking, and tree wells have created a less-cohesive streetscape
- Street trees are in decline in many cases
- Rainwater drainage capacity is insufficient
- Untreated rainwater degrades river quality
- Inadequate vehicular sightlines at intersection with East Alley
- Inadequate vehicular sightlines at intersection with Webb Street

## OPPORTUNITIES

- Integrate public art
- Reconfigure interfaces with East Alley and Webb Street
- Opportunities for infill development
- Rainwater features that enhance water quality and improve aesthetics
- Standardize historic district street fixtures and furnishings



SECONDARY FOCUS AREAS



Webb Street West is a two-way street that connects Mimosa Boulevard with Camp Avenue. Although it interfaces directly with historic downtown Roswell, it has a completely different character, consisting of detached single-family residences on large lots. The residents on this end of Webb have resided here for many years and wish to retain the low-density residential character of the street. Parallel parking is not allowed on the street.

ASSETS

- Strong connection to Historic Downtown Roswell
- Engaged community with strong vision
- Large open spaces
- Semi-rural scenic character
- No need for service access

CHALLENGES

- |  |  |
|--|--|
| <ul style="list-style-type: none"><li>• Development pressure from neighboring developments</li><li>• No formal pedestrian facilities</li><li>• Limited connectivity to surrounding neighborhoods</li></ul> | <ul style="list-style-type: none"><li>• Rainwater drainage capacity is insufficient</li><li>• Untreated rainwater degrades river quality</li></ul> |
|--|--|

OPPORTUNITIES

- Increase connectivity to surrounding neighborhoods
- Create formal pedestrian facilities using appropriate scale and materials
- Rainwater features that enhance water quality and improve aesthetics

IMPACT OF ROSWELL'S UNIFIED DEVELOPMENT CODE & DESIGN GUIDELINES

With the recent adoption of the new Unified Development Code (UDC) and the accompanying Design Guidelines, the City of Roswell has new regulatory tools to help direct new development and improvements to existing development. Many of the requirements of the UDC and Design Guidelines will apply to this Master Plan, among which are:

- |   |   |
|---|---|
| <ul style="list-style-type: none"><li>• Focus on pedestrian-oriented design and connectivity</li><li>• Address stormwater issues using Low Impact Development (LID) principles where possible</li><li>• Support Historic Preservation</li><li>• Provide outdoor public spaces that provide views of historic buildings or streetscapes, while minimizing the visual impacts on the historic streetscape</li><li>• Establish landscape to reduce heat island effect, minimize the visual impact of parking and service areas, establish visual continuity, and minimize water demand due to irrigation</li><li>• Use appropriate building materials with consideration for durability, local sourcing, and historic compatibility</li><li>• Promote active street frontage</li><li>• Consider public art</li><li>• Use site lighting with consideration for area identity, appropriate scale, and minimal off-site glare</li><li>• Minimize the visual impact of utilities, surface parking, and service areas</li><li>• Coordinate site furnishings and landscape with neighboring properties</li></ul> | <ul style="list-style-type: none"><li>• Engage the public realm on the street level with consideration for promoting pedestrian activity, incorporating traditional façade elements, designing a high level of transparency, and providing shelter from inclement weather</li><li>• Consider vehicular and bicycle parking requirements</li><li>• Respect height and setback requirements of the historic district</li><li>• Provide places for loading and unloading activities</li><li>• Comply with refuse regulations</li></ul> |
|---|---|



## PHOTOGRAPHIC ANALYSIS: ASSETS





## PHOTOGRAPHIC ANALYSIS: CHALLENGES





PHOTOGRAPHIC ANALYSIS: OPPORTUNITIES





ALLEYWAYS WORTH A SECOND GLANCE...

# BRIGHTLEAF SQUARE

DURHAM, NC



Photo: Mr. Granger

# BROWN STREET

GREENVILLE, SC



Photo: Alex Paulson

# COAST ALLEY

CHARLESTON, SC



Photo: Dottore Gianni



Photo: Caroline Culler



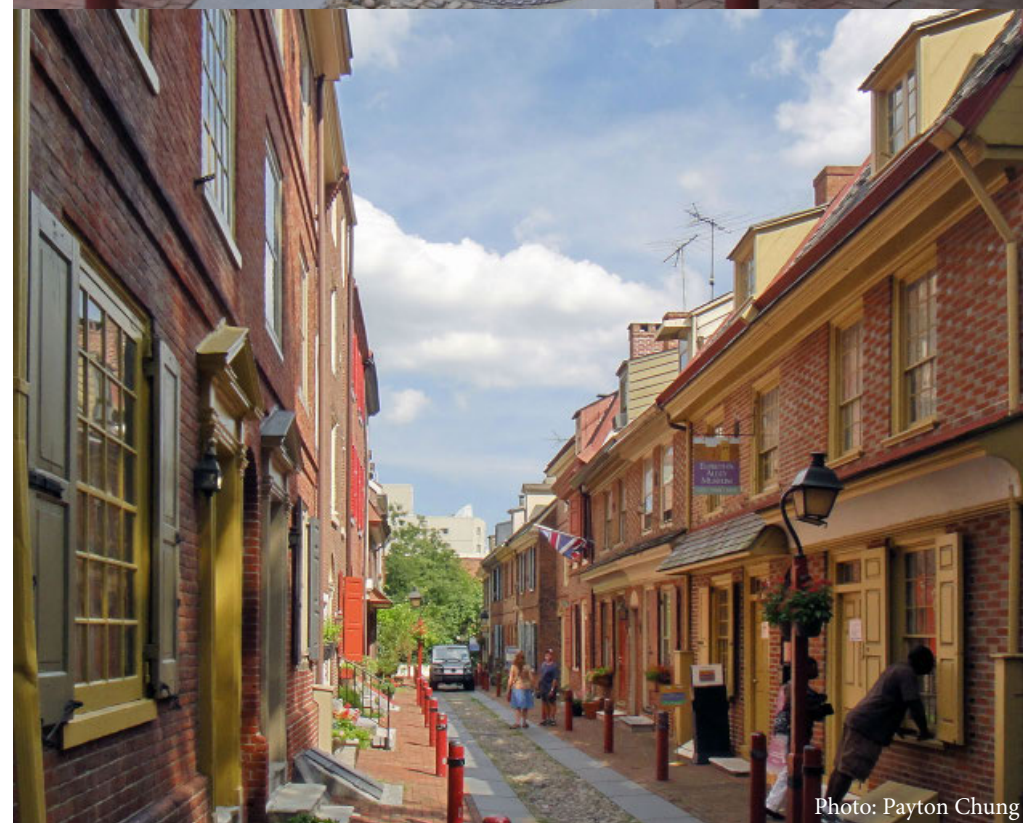
Photo: Alex Paulson



Photo: Coast Bar & Grill



## ALLEYWAYS WORTH A SECOND GLANCE...





COMMUNITY CHARRETTE





## COMMUNITY CHARRETTE

On June 17<sup>th</sup>, 2014, a public charrette was held at Roswell Historic Cottage, which was attended by approximately 90 people. The charrette was divided into two sessions, one in the morning and one in the afternoon. The morning session was primarily attended by business and property owners in the study area and consisted of focused design exercises and discussions that endeavored to build consensus around design ideas for the alleyways. The afternoon session was open to the general public and consisted of a presentation of the ideas generated in the morning design session, as well as a question and answer period.

In general, the challenges and opportunities identified in the inventory and analysis phase aligned closely with the issues brought up during the community charrette. The results of the focused design exercise revealed that the primary stakeholders were interested in creating a pedestrian-focused experience in historic downtown Roswell, incorporating many of the elements found in the analysis phase.

During both sessions, participants were asked to complete three survey worksheets as follows, the results of which are documented on the following pages:

1. Study corridor prioritization (page 20)
2. Improvement prioritization (page 21)
3. Visual preferencing (page 22-24)

Parking especially was considered to be the number one issue – both parking for business patrons and parking for employees. Because this master plan is focused primarily on right-of-way and city-owned land, it is not in the scope of this study to locate a large, structural parking solution for the downtown area.

There are several privately and publicly owned parcels in and around the study area that have high potential for redevelopment (see Figure 21). It is likely that future redevelopment will include parking solutions that could relieve some parking issues, both the perception of inadequate number of spaces, and the prevalence of surface parking lots which conflicts with a welcoming and pleasant pedestrian experience.



SURVEY 1: STUDY CORRIDOR PRIORITIZATION

SURVEY WORKSHEET

WHICH IS MOST IMPORTANT?

RANK THE FOLLOWING CORRIDORS BY INDICATING WHICH YOU FEEL SHOULD RECEIVE ATTENTION FIRST USING 1 AS THE HIGHEST PRIORITY AND 5 AS THE LOWEST, RANK EACH CORRIDOR ON A SCALE FROM 1 TO 5.

	HIGH PRIORITY				LOW PRIORITY
CANTON STREET	1	2	3	4	5
EAST ALLEY	1	2	3	4	5
ELIZABETH WAY	1	2	3	4	5
MIMOSA BLVD	1	2	3	4	5
WEBB ST EAST	1	2	3	4	5
WEBB ST WEST	1	2	3	4	5
WEST ALLEY	1	2	3	4	5

SURVEY RESULTS

RANKING	CORRIDOR
1	EAST ALLEY
2	WEST ALLEY
3	WEBB ST EAST
4	WEBB ST WEST
5	CANTON ST
6	ELIZABETH WAY
7	MIMOSA BOULEVARD

This survey was intended to gain an understanding of how the public perceives which of the study corridors should be prioritized for improvement. The aggregated results indicate that the charrette participants prioritize improvements to East Alley, West Alley, and Webb Street East over the other corridors in the study area.



# SURVEY 2: IMPROVEMENT PRIORITIZATION

## SURVEY WORKSHEET

WHICH IS MOST IMPORTANT?

RANK THE FOLLOWING ITEMS BY INDICATING WHICH YOU FEEL IS MOST IMPORTANT TO BE ACHIEVED. PLEASE WRITE-IN ADDITIONAL PRIORITIES THAT MAY NOT BE LISTED BELOW. USING 1 AS THE HIGHEST PRIORITY AND 5 AS THE LOWEST, RANK EACH ITEM ON A SCALE FROM 1 TO 5.

HIGH PRIORITY

→

LOW PRIORITY

INFILL DEVELOPMENT/ REDEVELOPMENT	1	2	3	4	5
DEFINE RESTAURANT/ RETAIL ALLEY INTERFACE	1	2	3	4	5
STORMWATER CONTROLS/GREEN INFRASTRUCTURE IMPROVEMENTS	1	2	3	4	5
PEDESTRIAN IMPROVEMENTS (SIDEWALKS/ LIGHTING)	1	2	3	4	5
ADD TO EXISTING PARKING/IMPROVE PARKING	1	2	3	4	5
VEHICULAR CIRCULATION AND SAFETY IMPROVEMENTS	1	2	3	4	5
GATEWAYS/ WAYFINDING/ BRANDING	1	2	3	4	5
PLACE OVERHEAD UTILITIES UNDERGROUND	1	2	3	4	5
INSTALL PUBLIC ART	1	2	3	4	5
CONSOLIDATE ALLEY SERVICES (TRASH, DELIVERIES, ETC)	1	2	3	4	5
_____	1	2	3	4	5
_____	1	2	3	4	5

## SURVEY RESULTS

- RANKING

IMPROVEMENT TYPE
- 1

2

3

4

5

6

7

8

9

10
- IMPROVE PARKING

ADD SIDEWALKS

SERVICE & DELIVERY

SAFETY & SIGHTLINES

UTILITIES

RAINWATER

BUSINESS CONNECTIONS

DEVELOPMENT

BRANDING

PUBLIC ART

This survey was intended to gain an understanding of which types of projects should take priority in the study area. The aggregated results show that the charrette participants prioritize improving parking, adding pedestrian facilities, and formalizing service/delivery areas over other types of improvements.





# SURVEY 3: VISUAL PREFERENCING

This survey was intended to poll the public as to which styles and materials they liked best when considering the aesthetics and needs of downtown Roswell. The aggregated results can be seen on the opposite page. In general, the preference among charrette participants was for metal and brick materials over wood and stone, and for traditional styles over more modern styles.

## SURVEY BOARDS

















PLEASE MARK PREFERENCES ON YOUR WORKSHEETS

WHICH DO YOU PREFER?

PAVEMENT MATERIALS	SEATING	PEDESTRIAN LIGHTING	DECORATIVE LIGHTING
<div>1. BRICK</div> 	<div>1. WOOD</div> 	<div>1. AMERICAN INDUSTRIAL</div> 	<div>1. TREE CANOPY LIGHTS</div> 
<div>2. STONE</div> 	<div>2. METAL</div> 	<div>2. COLONIAL</div> 	<div>2. TREE TRUNK LIGHTS</div> 
<div>3. PERMEABLE "GREEN"</div> 	<div>3. NATURAL STONE</div> 	<div>3. ACORN</div> 	<div>3. STRING</div> 
<div>4. PERMEABLE</div> 	<div>4. CUT STONE</div> 	<div>4. PENDANT</div> 	<div>4. POLE LIGHTS</div> 

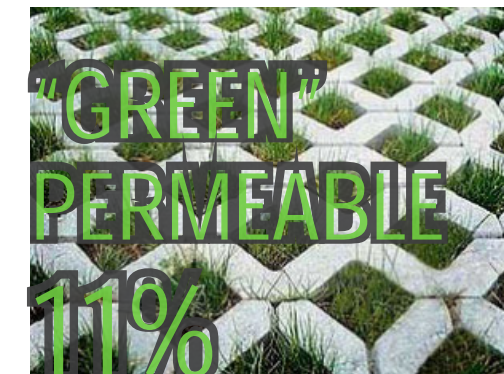
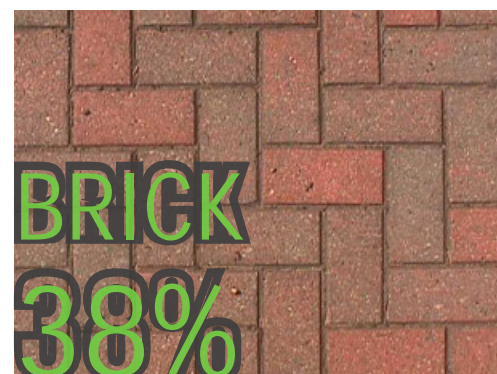
PLEASE MARK PREFERENCES ON YOUR WORKSHEETS

WHICH DO YOU PREFER?

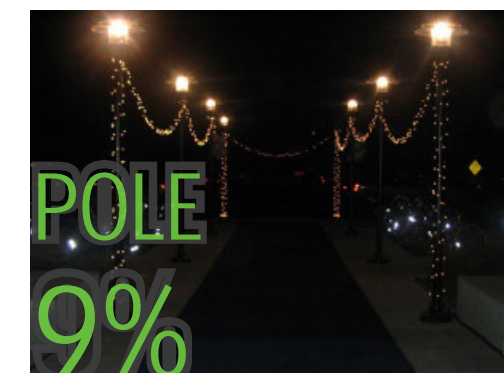
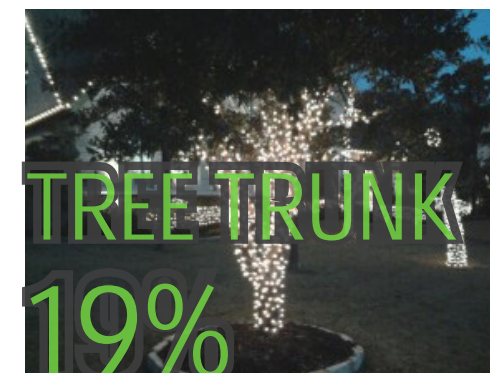
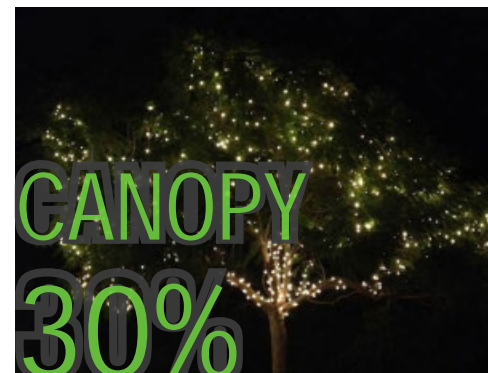
SCREENING	PLANTERS	BOLLARDS	RAINWATER
<div>1. WOOD FENCE</div> 	<div>1. STEEL</div> 	<div>1. MODERN ILLUMINATED</div> 	<div>1. STORMWATER PLANTERS</div> 
<div>2. METAL FENCE/BRICK</div> 	<div>2. CONCRETE RECTANGULAR</div> 	<div>2. TRADITIONAL ILLUMINATED</div> 	<div>2. PERMEABLE PAVEMENT</div> 
<div>3. METAL GATE/ WITH ROOF</div> 	<div>3. FIBERGLASS</div> 	<div>3. TRADITIONAL</div> 	<div>3. RAINGARDEN</div> 
<div>4. COMPOSITE FENCE</div> 	<div>4. ROUND</div> 	<div>4. MODERN</div> 	<div>4. BIOCELLS</div> 



## SURVEY 3: VISUAL PREFERENCING

PAVEMENT  
MATERIALS

## SEATING

PEDESTRIAN  
LIGHTINGDECORATIVE  
LIGHTING

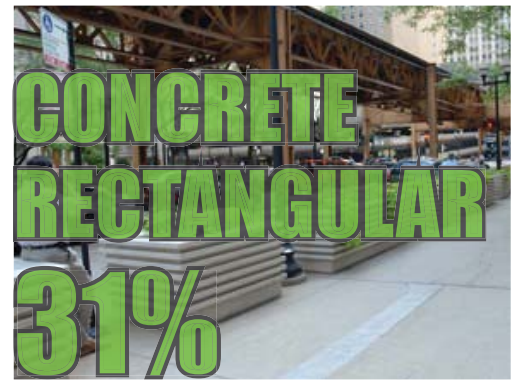


SURVEY 3: VISUAL PREFERENCING

SCREENING



PLANTERS



BOLLARDS



RAINWATER





# CONCEPT PLANS

Between June 17 and July 7, 2014, fifteen concept plans were generated based on the information gathered during the charrette and inventory/analysis phase. Three concepts were generated for Canton Street, East Alley, West Alley, and Webb Street East. Two concepts were generated for Elizabeth Way, and only one concept was completed for Webb Street West. No concept was presented for Mimosa Boulevard, as this streetscape was widely acknowledged to be complete – not needing a concept or approach different from its current condition.

These fifteen plans were presented to Roswell city staff in a meeting on July 7, 2014, to the public in an open house format on July 9, 2014, to the Roswell Historic Preservation Commission during their monthly meeting on July 9, 2014, to the Downtown Development Authority, and to Mayor and City Council during their public work session on July 14, 2014. Comments on these plans were gathered and compiled from all of these meetings and were used to make decisions to make a cohesive master plan going forward.



CONCEPT PLANS: CANTON STREET



Figure 5: Canton Street Concept 1



## CONCEPT PLANS: CANTON STREET



Figure 6: Canton Street Concept 2



CONCEPT PLANS: CANTON STREET



Figure 7: Canton Street Concept 3



## CONCEPT PLANS: EAST ALLEY



Figure 8: East Alley Concept 1



# CONCEPT PLANS: EAST ALLEY



Figure 9: East Alley Concept 2



## CONCEPT PLANS: EAST ALLEY



Figure 10: East Alley Concept 3



# CONCEPT PLANS: WEBB STREET EAST



Figure 11: Webb Street East Concept 1



## CONCEPT PLANS: WEBB STREET EAST



Figure 12: Webb Street East Concept 2



# CONCEPT PLANS: WEBB STREET EAST



Figure 13: Webb Street East Concept 3



## CONCEPT PLANS: WEST ALLEY



Figure 14: West Alley Concept 1



# CONCEPT PLANS: WEST ALLEY



Figure 15: West Alley Concept 2



## CONCEPT PLANS: WEST ALLEY



Figure 16: West Alley Concept 3



# CONCEPT PLANS: ELIZABETH WAY



Figure 17: Elizabeth Street Concept 1



## CONCEPT PLANS: ELIZABETH WAY



Figure 18: Elizabeth Street Concept 2



# CONCEPT PLANS: WEBB STREET WEST



Figure 19: Webb Street West Concept 1



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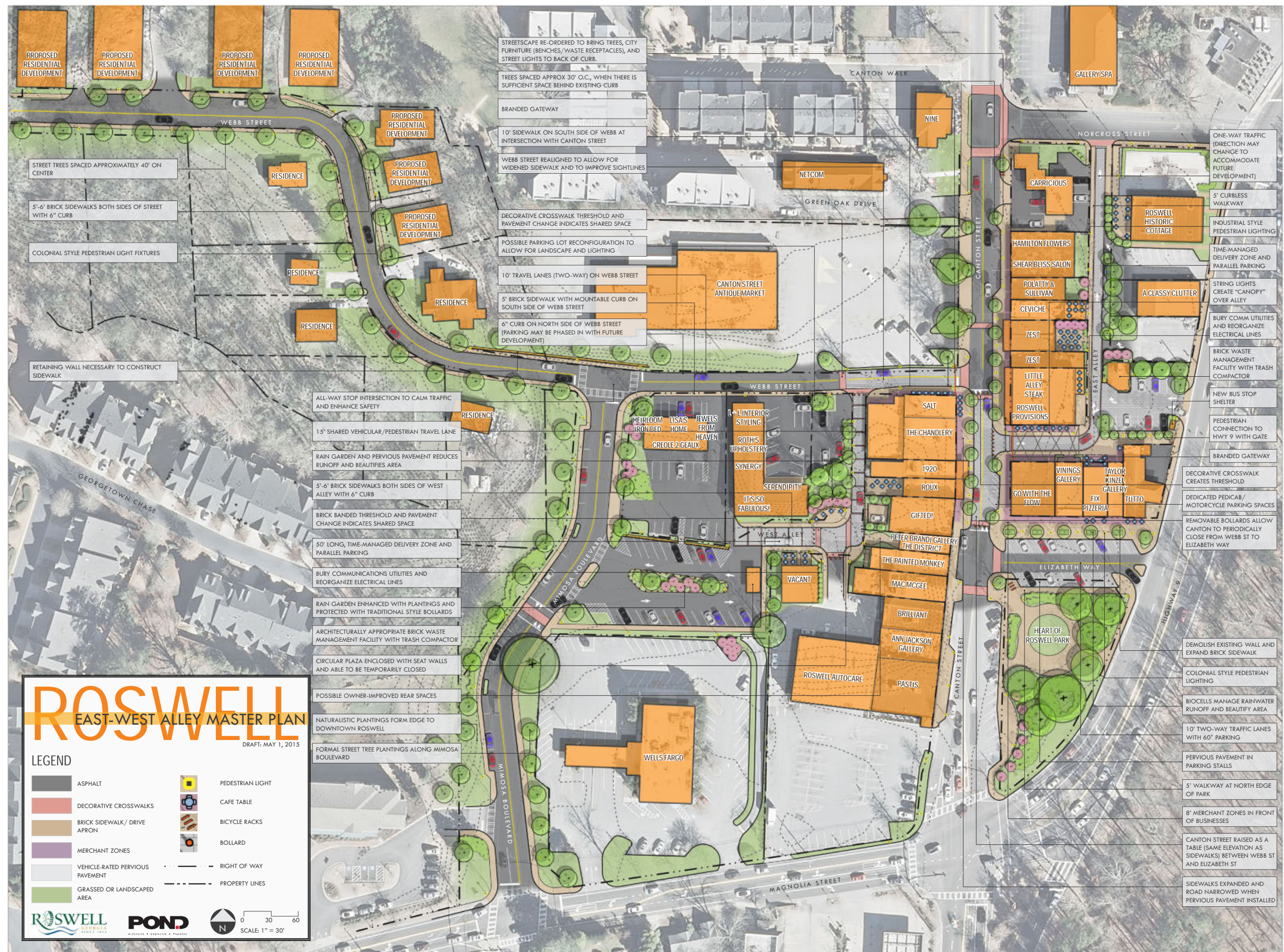


Figure 20: Final East-West Alley Master Plan



## MASTER PLAN

Feedback on the fifteen concept plans was collected from the public, elected officials, and city staff and synthesized into one cohesive master plan. The draft master plan was presented to the Historic Preservation Commission on August 13, 2014, to the Downtown Development Authority on August 14, 2014, and to Mayor and City Council during their work session on August 25<sup>th</sup>, 2014. The public also got two chances to comment on the plans at a public open house on August 19<sup>th</sup>, 2014 and during a public presentation during the popular monthly “Alive After 5” event in historic downtown Roswell on August 21<sup>st</sup>, 2014. Comments were collected from these presentations, and changes made to the draft master plan based on those comments.

On the opposite page is the final master plan (Figure 20), which takes into account all comments received from the draft planning effort. Also included are plan enlargements of the east and west zones of the study area (Figures 21 and 22) and a plan which shows parcels which may potentially redevelop in the future (See Appendix A). Conceptual renderings (Figures 23-26) were also developed that show before and after snapshots of the downtown area if recommended improvements are implemented. Note that all of these plans have been resized to fit into the format of this report. Full-sized copies are available for viewing at the City of Roswell Community Development Department.

In general, there is strong interest in improving the pedestrian experience throughout the downtown area among the public, city staff, and elected officials. However, the parking issue continues to hold sway in the minds of many. This makes it challenging to suggest improvements that will reduce the number of parking spaces without also suggesting a large-scale parking solution alongside the corridor improvements. Aggregated opinions collected from the public suggest that there is a sharp divide between those who wish to see a change in materials only versus those who want to see more functional change. Although more members of the public selected functional change over a material “facelift,” the small sample size makes extracting this result to the general population problematic.

All groups (the public, staff, and elected officials) responded favorably to the idea of time-managed closure of certain corridors, especially Canton Street. However, the possibility of closing Canton Street part-time is predicated on traffic being able to be diverted to side streets during times of closure, namely Webb and Elizabeth. Permanent closure of Canton Street did not garner a favorable reaction, and was removed from consideration.

Canton Street is the corridor considered by many stakeholders to need the least amount of attention, due to its commercial success. This plan makes two major recommendations for Canton Street which will increase its pedestrian appeal and promote its continued economic prosperity. The first recommendation is to manage the length of Canton Street between Webb St and Elizabeth St as a time-managed

closed street. The street itself is repaved with permeable pavers and raised to the same level as the sidewalk to promote traffic calming and a sense of shared space when the street is open to through traffic. Sidewalks will also be widened in this area.

The second recommendation for Canton Street is to reorder the sidewalks north of Webb Street behind the existing curb to achieve a consistent streetscape order, with a 5-foot wide zone for city furnishings, street lights, and street trees, a 6-foot clear walkway, and an 8-foot commercial merchant zone. This will give a coherent rhythm to the corridor, within which the character and identity of Canton Street’s businesses can develop.

East Alley may be the most complicated alley for which to make recommendations, based on the multiple functions and interests involved. Generally, the consensus is that cleaning up East Alley by burying utilities, improving pavement treatments, and adding lighting would have a positive impact on the corridor. Additionally, consensus seems to be in favor of changing the circulation to one-way to reduce vehicular and pedestrian conflicts while allowing space for pedestrian and landscape improvements in the right-of-way. The idea of a waste consolidation building was also met with general approval. Mayor Jere Wood voiced a strong opinion during the work session that the east side of East Alley ought to be redeveloped so that the corridor becomes a two-sided street, which would increase commercial and social activation and extend the Canton Street experience. A gateway feature that “brands” the alleyway also garnered considerable support among all groups.

The consensus for West Alley seems to be that the idea of a communal space of some kind may reduce the perception of a dark and messy alleyway, as long as it is activated. As with East Alley, general consensus agrees that cleaning up the alley through pavement treatments, utility burial, pedestrian lighting, and landscaping would have a positive impact on the pedestrian experience and would accelerate business improvements to their rear spaces. Vehicular circulation needs to be maintained to the rear of all the buildings in the alley. Although city staff liked the idea of using a large existing building for consolidated waste management, there was considerably less support for this idea among members of the public, who see more commercial potential in this building. The circular form seemed to resonate and generate more excitement than the other options in the concept plans phase (see Figure 15). However, the circle is modified in the final plan to provide for a more urban pedestrian-scale experience, rather than acting as a traffic circle.

Webb Street East presents a dilemma, as existing private parking conditions and property ownership limits the improvements that can fit inside the existing 23-25’ right of way. In order to make recommended improvements to this corridor, it will be necessary to acquire a wider right of way. This corridor also has great potential to extend the Canton Street experience westward. With current development patterns,

it will be difficult to achieve this however, due to the large areas that are given to surface parking, and the lack of any sort of pedestrian facilities on either side of the street. The idea of creating a “mirror image” to East Alley, including a pavement treatment and branded gateway, is a compelling one, and garners support among all groups. Protective attitudes about existing parking will make it challenging to develop this corridor in the absence of a larger redevelopment that includes the surrounding parcels (See Appendices A & C).

Elizabeth Way currently does not offer the same caliber pedestrian experience as Canton Street. It essentially functions as a parking lot with an extra wide drive lane. In this plan, Elizabeth Way is narrowed and converted to a two-way street to allow for the periodic closure of Canton Street north of Elizabeth Way. There is also consensus for expanding the sidewalks and merchant zones along the existing row buildings, while eliminating the existing brick wall that essentially acts as a barrier in the middle of the sidewalk.

Webb Street West remains a residential street, but incorporates streetscape treatments including sidewalks, pedestrian lighting, and street trees. The treatment in this area may be less intense than the rest of the study area in keeping with its quieter nature and residential land use.





Figure 21: West Enlargement





Figure 22: East Enlargement



# CANTON STREET CONCEPTUAL RENDERING

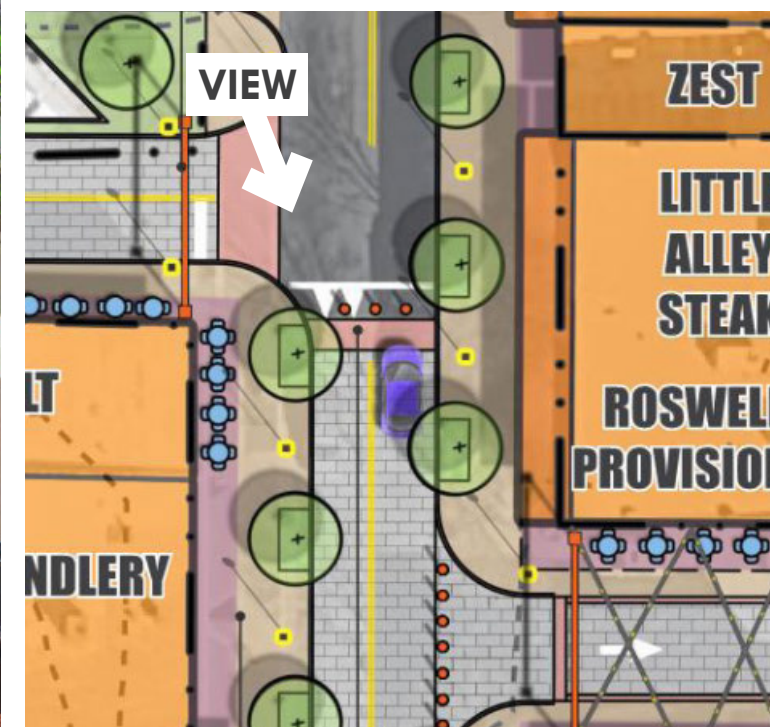


Figure 23: Canton Street Conceptual Rendering



# EAST ALLEY CONCEPTUAL RENDERING

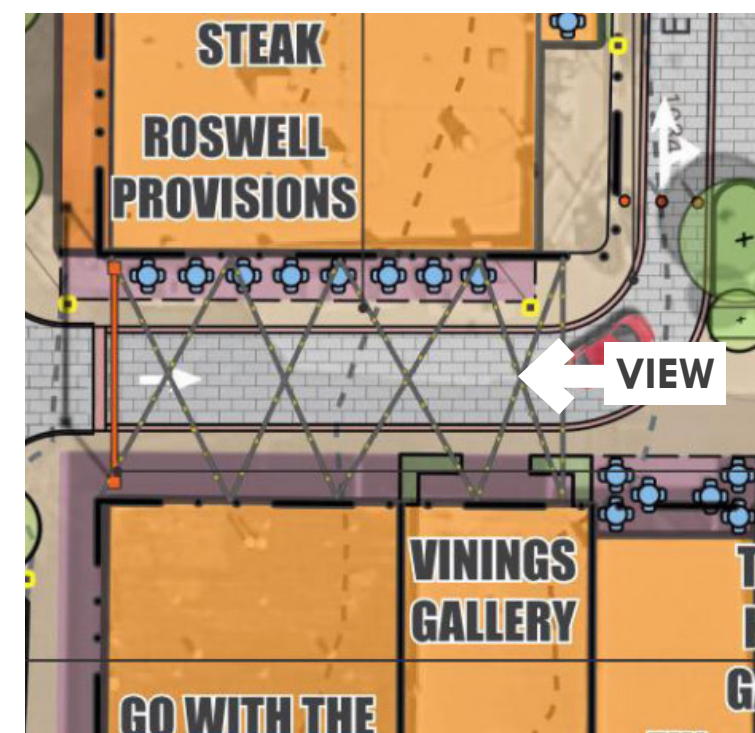


Figure 24: East Alley Conceptual Rendering



# WEBB STREET CONCEPTUAL RENDERING

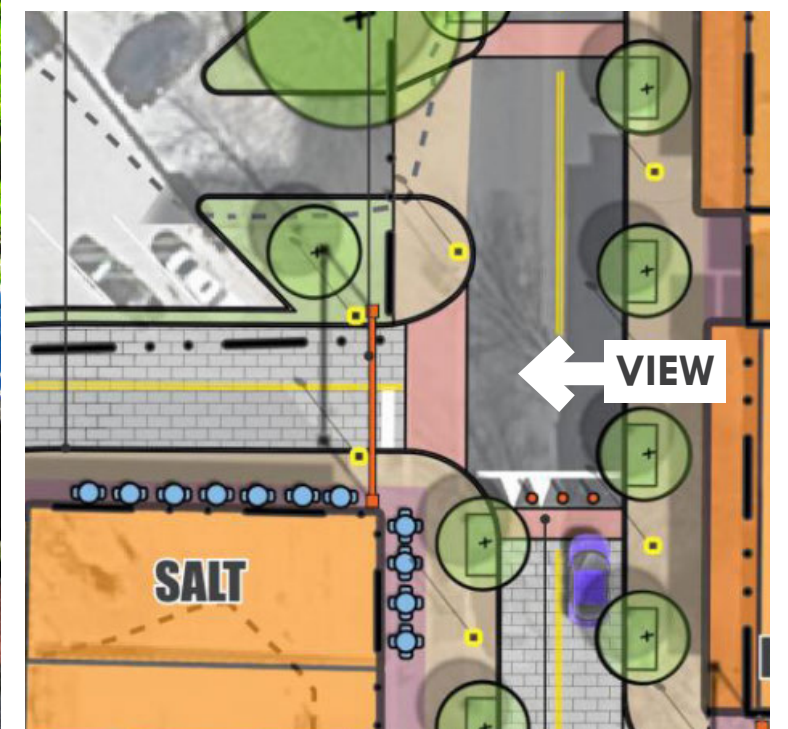
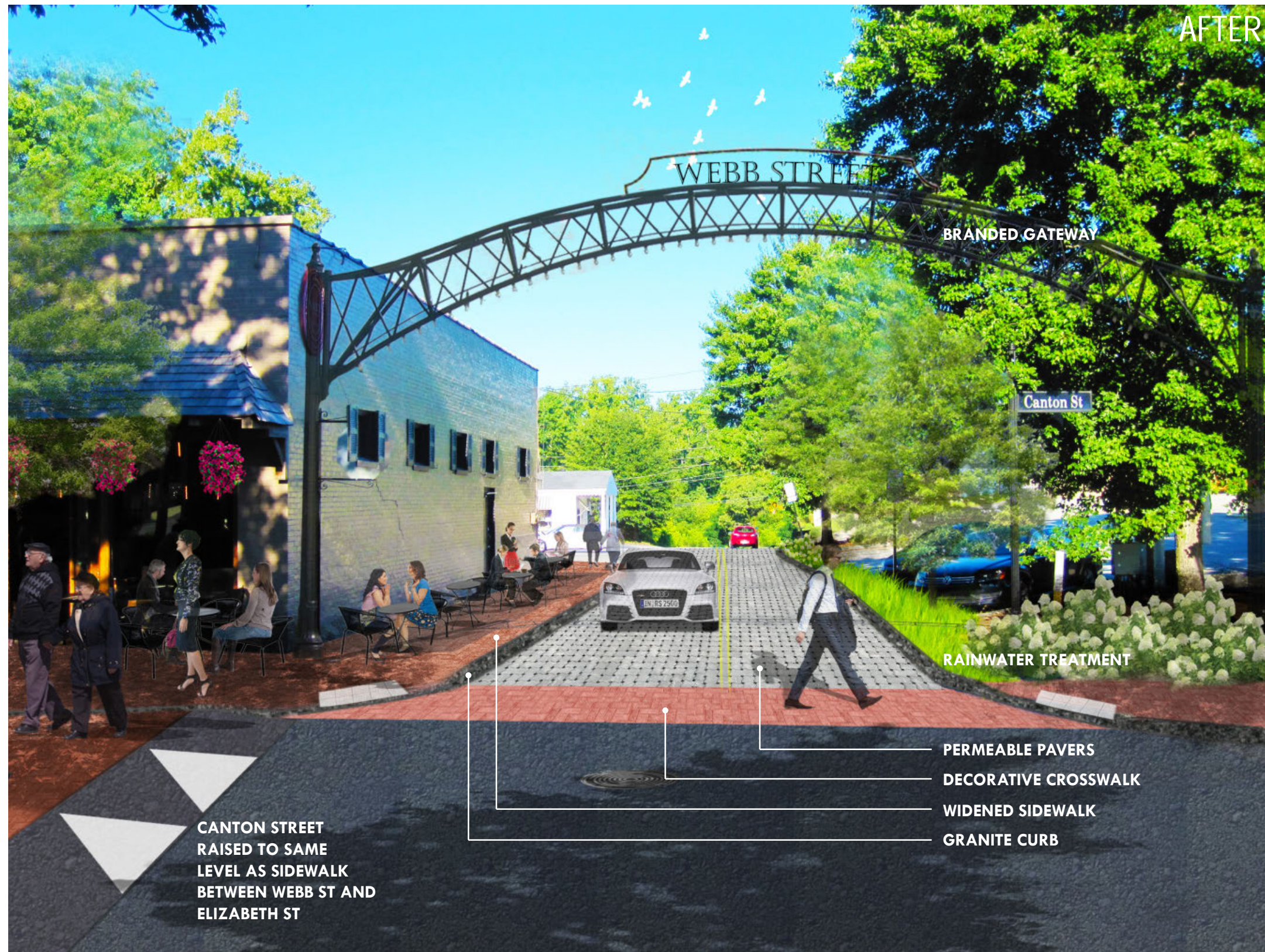


Figure 25: Webb Street East Conceptual Rendering



# WEST ALLEY CONCEPTUAL RENDERING

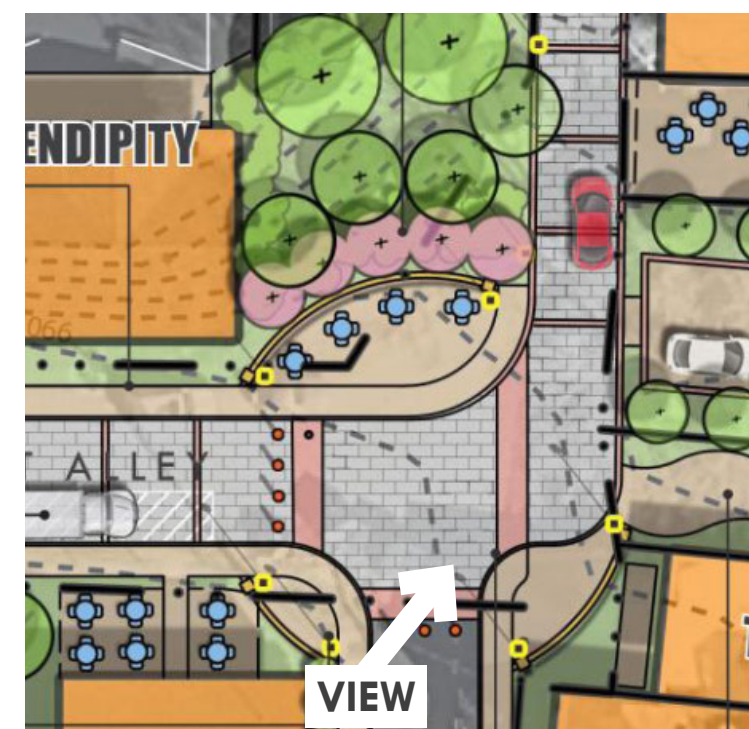


Figure 26: West Alley Conceptual Rendering







## DESIGN STANDARDS

The design standards set forth in this chapter are intended to standardize streetscape elements and materials the East-West Alley Master Plan study area. The primary reason for creating these design standards is to improve and unify the visual aesthetics of Downtown Historic Roswell.

The standards outlined on the following pages are based on three main sources: feedback from the June 17<sup>th</sup> Community Charrette, a visual survey of the existing conditions in the study area, as well as a review of existing standards for adjacent areas in the City of Roswell.

Any roadway project or private development project that is required to make improvements in the public right-of-way will need to comply with these standards in addition to complying with all other applicable local and state laws.



BENCH



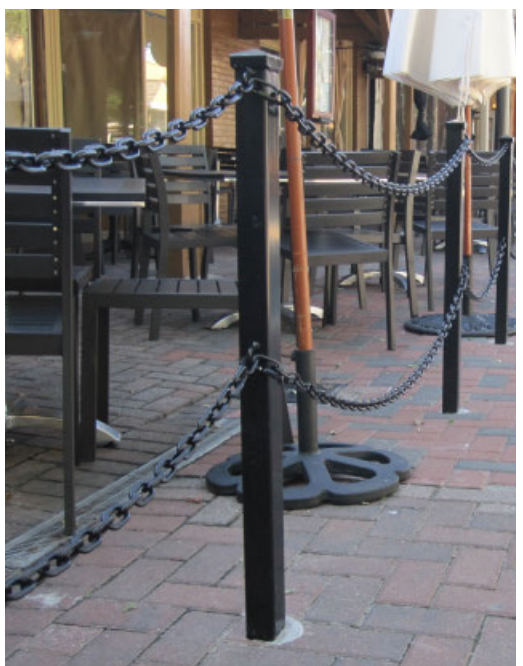
Victor Stanley  
Classic Series  
CBF-10  
6-Foot length  
Horizontal steel slats  
Surface mount  
Optional: Intermediate arm rest  
Black powder coat finish

WASTE/RECYCLING RECEPTACLE



Victor Stanley  
Ironsites Series  
SD-242  
41-3/4 - Inch height  
28-Inch top diameter  
Vertical steel slats  
Surface mount  
Black powder coat finish  
One side for refuse and one side for recycling

RESTAURANT POST



2” square hollow structural section steel bollard embedded in walkway.  
Black finish with hipped bollard cap.  
Two lengths of black powder-coated steel chain strung between bollards

BUS SHELTER



Daytech Limited  
Products:  
“Palladian” Bus Shelter  
“Easy-Access” Bench  
Map Frames  
Color: Black  
Glass to have white safety dots

BICYCLE RACK



Trilary Inc. (Madrax)  
‘R’ Bike Rack  
Surface mount  
Black powder coat finish



DESIGN STANDARDS

LIGHTED BOLLARD



- Amerlux Exterior
- DB39
- Heavy wall cast aluminum
- Traditional fluted styling
- Clear acrylic lens
- Black powder coat finish

REMOVABLE BOLLARD



- Reliance Foundry
- R-7539
- Ductile Iron Fluted Bollard
- Black powder coat finish

PEDESTRIAN LIGHT LUMINAIRE - COLONIAL STYLE

This style of luminaire should be used in all streetscapes in historic downtown Roswell, with the exception of East Alley and West Alley. It is stylistically similar to what currently exists in the streetscapes along Mimosa Boulevard and Canton Street, but new light fixtures will be full cutoff. Full cutoff is a term used to describe luminaires that emit no uplight, or light emitted above horizontal.

Features

- 6 Sided lantern luminaire
- Clear acrylic lens
- 100 Watt High Pressure Sodium
- Color: Black powder coat finish
- Pole to match existing on Canton Street

Manufacturer

Main Street Lighting, Inc.  
Luminaire: L520R



Pedestrian light components: Luminaire and pole - image represents general style, but not the exact fixture.

PEDESTRIAN LIGHT LUMINAIRE - INDUSTRIAL STYLE

This style of luminaire should be used in East Alley and West Alley to differentiate the experience and “brand” the alleyways. The fixture shall be a full cutoff fixture in compliance with Dark Sky standards

Features

- Spun aluminum bell-style shade with flared edge
- Aluminum arm bolted to a post-mount adapter, welded to the pole
- 150 Watt High Pressure Sodium
- Color: Black powder coat finish
- Pole shall be Hapco Grand Series Lighting Pole

Manufacturer

Streetworks - Cooper Lighting  
CEL Classical Epic



Pedestrian light components: Luminaire and pole - image represents general style, but not the exact fixture.



### TREE GRATES

In an effort to both protect the roots of the trees, and increase walkable space on the public sidewalk, it is recommended that all street tree plantings within a sidewalk receive a tree grate.

A traditional radial design is recommended for Downtown Roswell.

**Manufacturer (choose one)**

Neenah Foundry

Extended Collection: R-8815-B

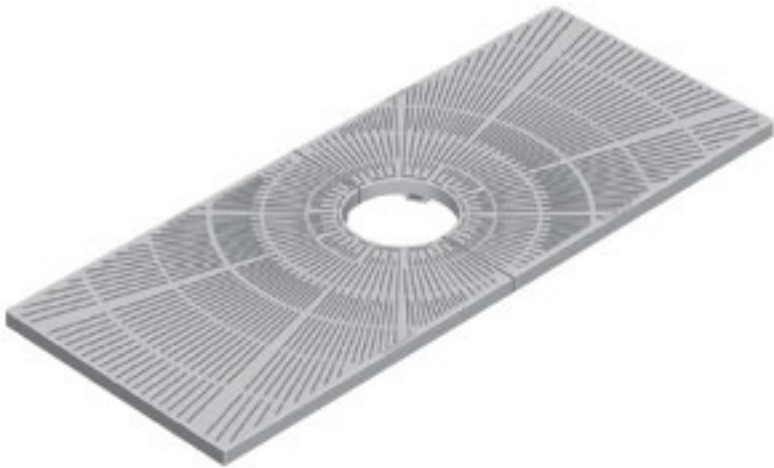
48” x 120”

OR

East Jordan Iron Works

8695

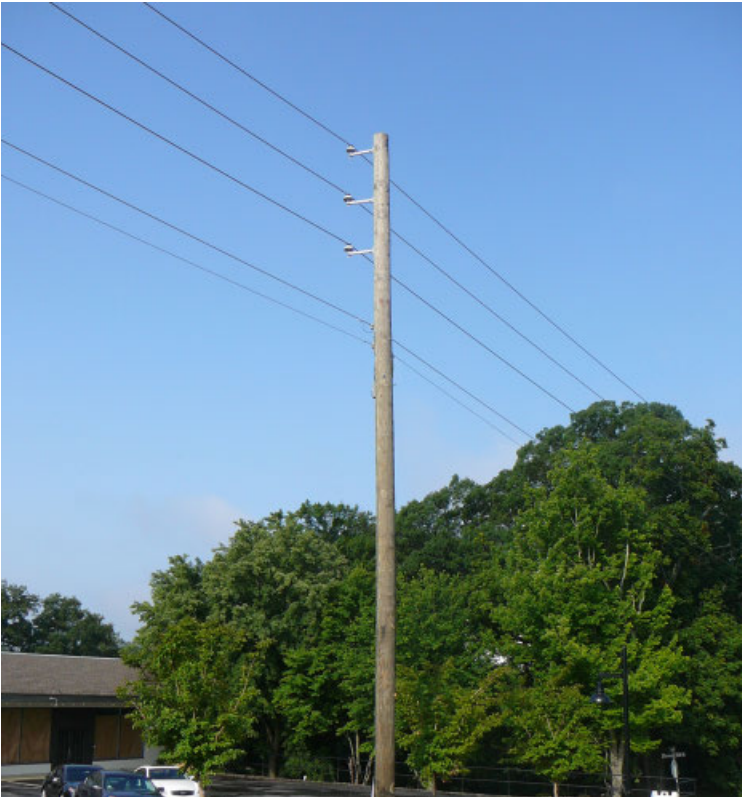
48” x 72”



### UTILITY POLE CHARACTER

Ideally, all utilities in the study area should be buried underground. However, this may not be feasible considering the cost associated with both electrical and communications line burial.

In these cases, communications lines may be buried to eliminate unsightly pole-mounted boxes. Electrical lines may be kept above-ground, but raised to a higher elevation and organized to allow for understory trees to be planted beneath the lines.



Example of clean power pole installation

### TREE BOXES

The City of Roswell recognizes the value of managing stormwater quality and quantity with a combination of traditional and low-impact infrastructure. Pre-fabricated planter boxes, such as the model detailed below, may be used as site conditions allow.

Care must be taken when selecting these planter boxes, as they are sized to accommodate small understory trees only. If larger trees are desired, then alternate solutions will need to be explored, such as ordering a deeper, larger planter box or using curb cuts to create custom rainwater management cells.

**Manufacturer**

Filtrerra Bioretention Systems





## DESIGN STANDARDS

**STREET TREES**

Street trees perform several important functions in the public right-of-way in Historic Downtown Roswell. They provide shade to pedestrians, lend character, and establish rhythm along the length of the street. Whenever conditions allow, overstory street trees should be planted in the sidewalk at 30' on center. Overstory trees shall be a minimum of 4-inch caliper and limbed up to a minimum height of 7 feet. Understory trees may be used when overstory trees are not able to be planted due to space constraints or overhead utility conflicts. The species listed below are trees that suit the character of Historic Roswell and will be drought and heat tolerant, requiring minimum maintenance.

**Overstory tree selections**

These trees should be planted behind the curb in designated planting and furniture zones. These species may also be planted in buffer zones and rainwater management areas.



Oak  
*Quercus sp.*



Elm  
*Ulmus sp.*  
(Princeton Elm pictured)



Red Maple  
*Acer rubrum*

**Understory tree selections**

These trees should be planted when overstory tree selections cannot be planted due to space constraints or overhead utility clearance. These species may also be planted in buffer zones and rainwater management areas.



Serviceberry (single-stem)  
*Amelanchier arborea*  
designated small street tree



Hybrid Flowering Magnolia  
*Magnolia x soulangeana*  
supplemental small tree



Eastern Redbud  
*Cercis canadensis*  
supplemental small tree



BRICK SIDEWALK

Areas devoted entirely to pedestrian activity, whether grade-separated or not, shall be paved with 4x8 red brick material in a 90° herringbone pattern with a soldier border at all edges. Pervious options may be used if desired.

Pine Hall Brick

Georgian Edge Full Range Color; straight edge with lugs

4-inch x 8-inch clay paver

OR

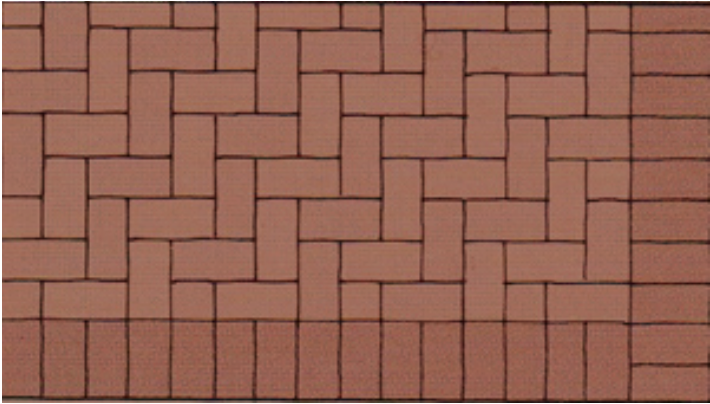
Rainpave Full Range Color

4-inch x 8-inch water permeable clay paver

Pavestone

Old Town Blend

4-inch x 8-inch cast stone paver



Paving pattern: 90 ° herringbone bond with soldier course border



Brick color range

VEHICLE RATED PERVIOUS PAVERS

There are several areas in the study area that call for vehicle-rated pervious pavers. These are applied in areas of shared vehicular/pedestrian use, parking stalls, and alleyways. They are meant to have a traffic-calming effect by indicating a shared pedestrian use as well as helping to manage stormwater.

Typically, pervious pavers are used in areas of lower traffic volumes and weights. However, due to the growing number of installations and ongoing research, the range of appropriate applications is expanding. Working installations exist in busy parking lots, public roads, and intersections. Proper installation techniques and maintenance are key to ensuring the long-term success of pervious paver applications.

As always, selection of pervious pavers should be based on engineering constraints, such as proximity to building foundations, infiltration rates of native soils, and expected vehicle loads. Some more decorative pavers may be susceptible to shifting than those designed to bear heavy-duty loads.

Heavy-duty style pavers designed for commercial and industrial uses are recommended here.

Pavestone

Uni-Ecoloc; Chicago Blend

9-inch x 4 1/2-inch permeable concrete paver



Heavy-duty pervious paver



Paver color range and typical interlocking pattern

CONCRETE BANDING BETWEEN BRICK AND VEHICLE PAVERS

In East Alley and West Alley, there is no curb or other grade separation between the brick sidewalk and the vehicle rated pervious pavers. In those cases, the two pavements shall be separated by a band of poured-in-place white concrete, as shown below. No additional concrete band will be necessary when a curb or other grade separation is present.



General scale of concrete band when grade separation is not present. Note that the pavement patterns shown below do not comply with these standards. This image is intended to show the relative size and color of the concrete band. Paver colors and patterns shall comply with their respective standards.

GRANITE HEADER CURB

Whenever curb is constructed or replaced in the historic district, a granite header curb shall be used, rather than concrete curb and gutter. There are numerous sources for locally-sourced granite curb in the state of Georgia.





## DESIGN STANDARDS

**DETECTABLE WARNING TILE**

All curb ramps shall be equipped with detectable warning pavers that contrasts with the surrounding pavement. Detectible warnings should have coloration that suggests natural material. At no time shall they be colored bright yellow. Refer to GDOT and ADA standards for other requirements.

**SPEED TABLE CHARACTER**

Speed tables may be constructed using asphalt and/or concrete, which will be colored to match the surrounding brick color, and top-stamped with a 90 degree herringbone bond pattern with soldier course border.

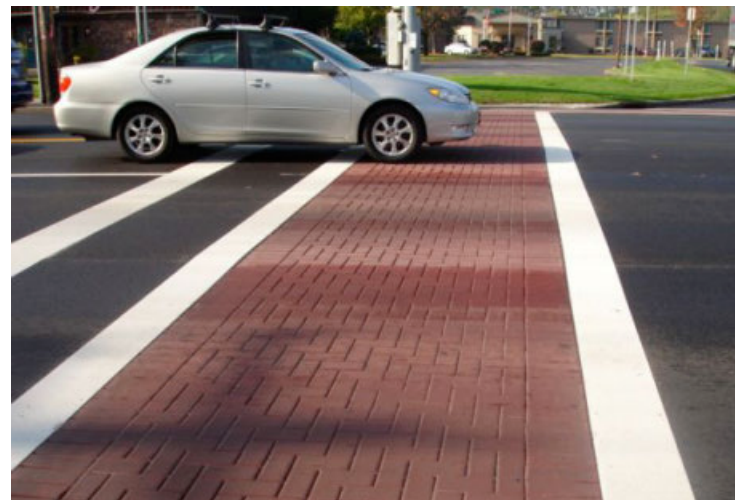
It is currently against city policy to install speed tables on Canton Street, due to its classification as a collector street. It is the recommendation of this master plan that an exception be considered in the study area due to the high volume of pedestrian traffic.



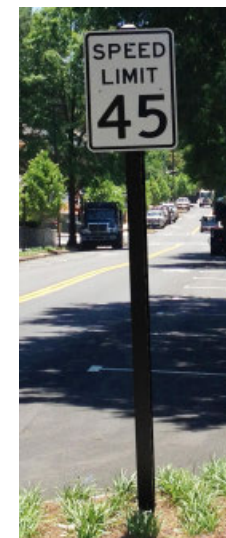
Asphalt speed table with stamped asphalt top and appropriate warning markings

**DECORATIVE CROSSWALKS**

Decorative crosswalks will be colored stamped asphalt with the same paving pattern as the brick sidewalks, a 90° herringbone pattern with a 8-inch soldier course border at all edges. Crosswalks should also include a 12-inch with thermoplastic band framing the stamped asphalt.

**TRAFFIC SIGNAGE**

MUTCD signage is important for motorist and pedestrian safety in any public street. This plan recommends a black powder-coated sign post and a black ElectroCut film backing and 1.5-inch black border.

**RETAINING WALLS**

Retaining walls installed to facilitate sidewalk and other improvements should have the character of a mortared granite rubble wall.

**WAYFINDING SIGNAGE**

Wayfinding signage should be incorporated into the streetscape at intersections and other points at which motorists or pedestrians reach a decision-making point. Destinations are limited to public facilities and areas.





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## APPENDIX A: AREAS FOR POTENTIAL REDEVELOPMENT

The exhibit on the following page illustrates the many redevelopment opportunities in and around the study area. Although this report was limited to making recommendations for improvements inside the existing right-of-way, opportunities for redevelopment extend into the surrounding parcels, especially in areas with large surface parking. It is possible that some of the recommendations made in this report may be implemented as part of a more comprehensive redevelopment in the downtown area that may include the parcels shown.







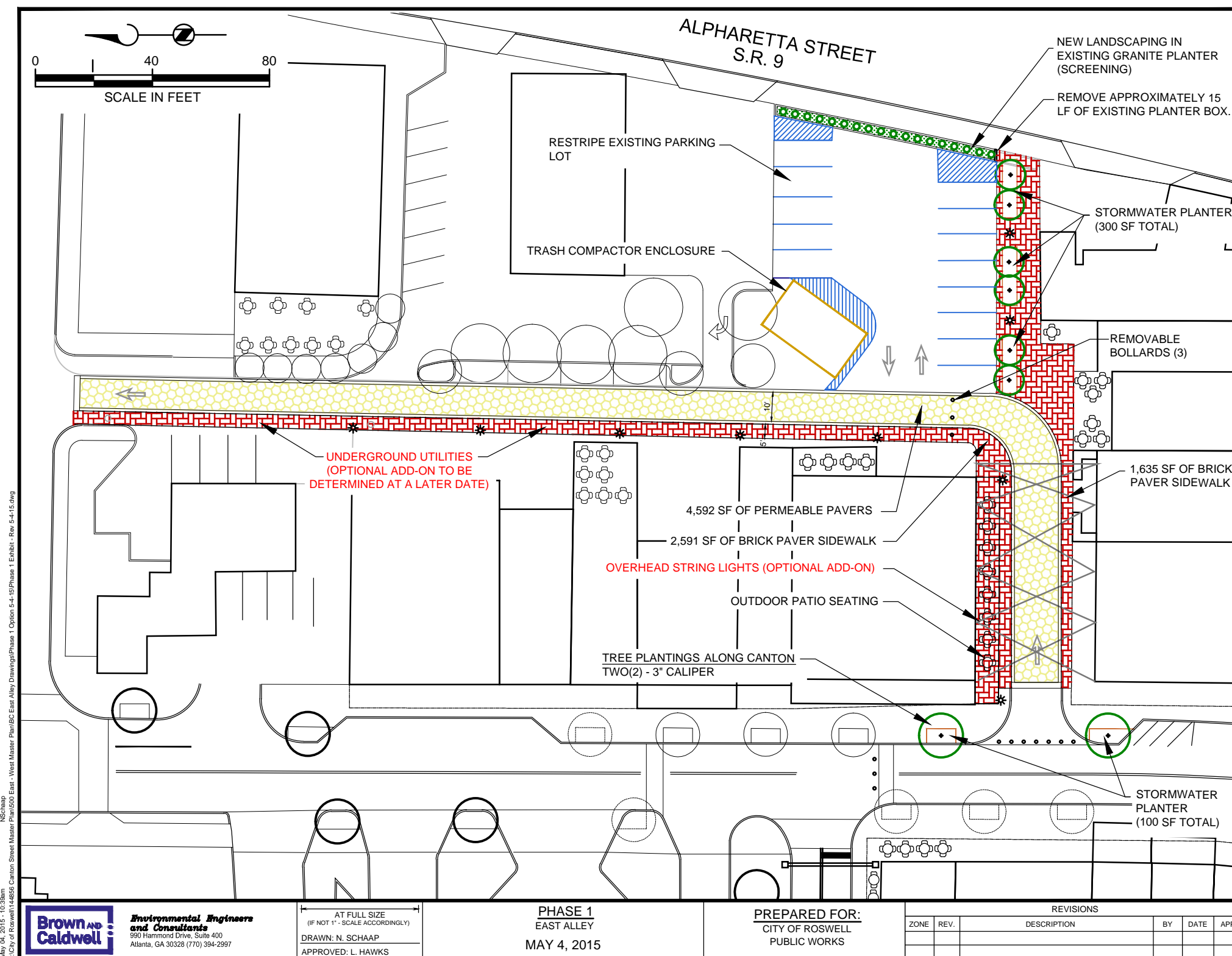








## APPENDIX B: PHASE I IMPLEMENTATION



At the time of this writing, the City of Roswell was exploring the phased implementation of the East Alley recommendations, including pavement replacement, utility burial, pedestrian lighting, parking lot restriping, landscape, and waste management improvements. A phase I schematic plan, based on the master plan, was developed by Brown & Caldwell under separate contract with the City of Roswell Department of Public Works and is included here as an appendix exhibit.

Two illustrative views are also included, showing the conceptual “before and after” of phase I including full utility burial.

It is important to note that elements contained in the master plan are not necessarily included in the Phase I implementation plan, including the braded gateways, new parallel parking, and bus shelter. Their exclusion in phase I does not preclude them from being implemented in future phases.

Figure 28: Phase I implementation of East Alley Improvements



## APPENDIX B: PHASE I IMPLEMENTATION



Figure 29: East Alley Phase I conceptual rendering with full utility burial (rendering by Pond & Company)



## APPENDIX B: PHASE I IMPLEMENTATION



Figure 30: East Alley Phase I conceptual rendering with full utility burial (rendering by Pond & Company)



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## APPENDIX C: FUTURE DEVELOPMENT OF WEBB ST EAST

Because the existing right-of-way on Webb Street East is only 25 feet wide, many recommended improvements for this corridor were left off the final version of the master plan. Currently the street is dominated by large amounts of surface parking with no formal pedestrian facilities. As properties surrounding Webb Street East redevelop, it is recommended that the city negotiate for a widened right-of-way to allow for a higher order of streetscape treatment and road realignment, including generous sidewalks, landscape, parallel parking, and stormwater management similar to the illustration below.

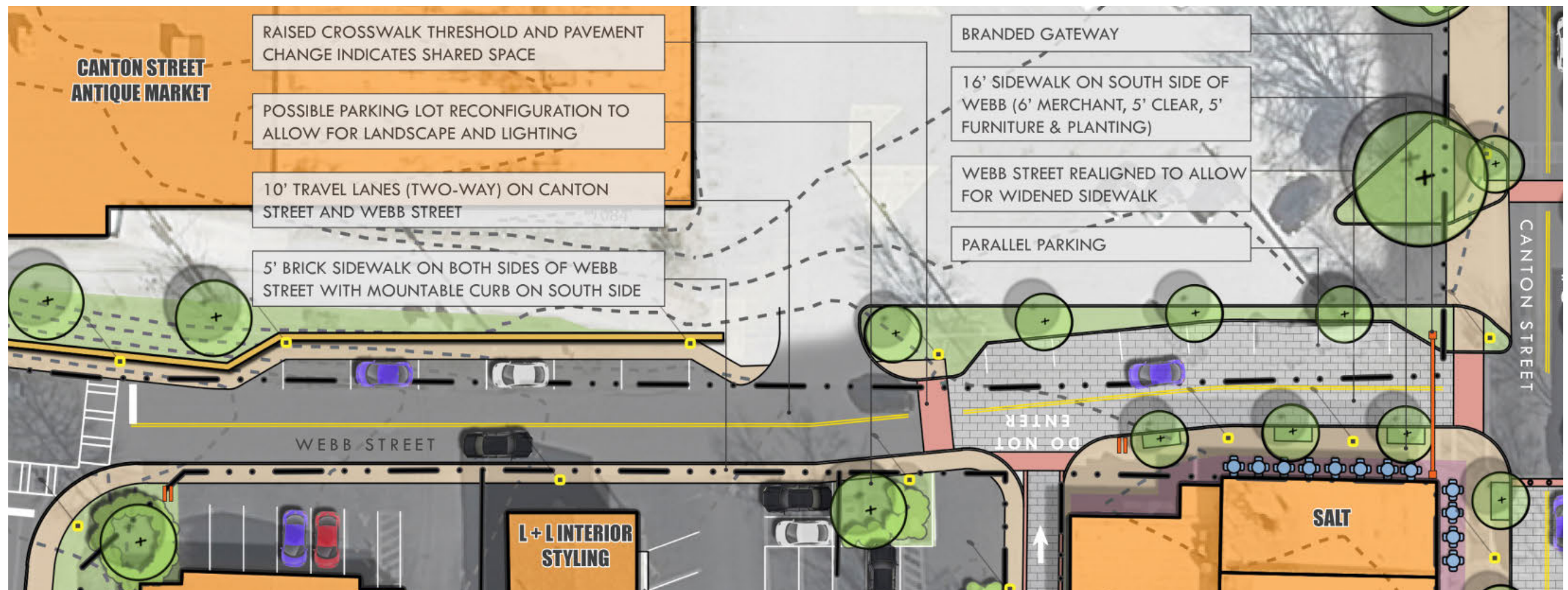


Figure 31: Webb Street East with improvements that would require a wider right-of-way